



draft Hotel Dunchoille Development Framework

Supplementary Planning Guidance November 2017

Contents

Hotel Dunchoille Development Framework

1. Introduction3

2. Site Overview3

3. Planning Policy Context3

4. Surrounding Area4

5.Immediate Area5

6. Site Description7

7. Site Analysis10

8. Development Guidelines12

9. Waste Management Plan.....14

IDP Policy Relevance.....15

1. Introduction

1.1. This Development Framework provides planning guidance for the potential residential development of Hotel Dunchoille, Guelles Road, St Peter Port. It provides broad, comprehensive and practical guidance on how the policies in the Island Development Plan will be applied to the site, and considers appraisal of the site and wider area. Once approved, this Development Framework will become Supplementary Planning Guidance and any planning application for residential development on the site should be in accordance with it.

2. Site Overview

2.1. The site is located in St Peter Port, within the Main Centre Outer Area. The site area is approximately 0.13 hectares (1 vergée). The site is accessed directly from Guelles Road. The surrounding area is built-up, predominantly residential, though adjoining the site to the east is an industrial area. Image 1 shows the site outlined in red on an extract from the Island Development Plan Proposals Map.

3. Planning Policy Context

3.1. In the Island Development Plan (IDP), this hotel site is located within the St Peter Port Main Centre Outer Area. The site has a long planning history and there is a current planning permission (which expires on 16th June 2018, planning reference FULL/2015/0988) for residential use granted under previous policies in the Urban Area Plan, prior to the adoption of the IDP. This granted approval for the conversion of the hotel into 9 flats, including demolition of part of the building.

3.2. There is a general policy presumption against the loss of visitor accommodation other than in exceptional circumstances or where operation is not financially viable, however whilst the loss of tourist accommodation has been established on the site, through the grant of the previous planning permission in these particular circumstances residential use could be accepted as a minor departure from the IDP.

3.3. However upon expiry of the existing planning permission, on 16th June 2018, if works have not been substantially commenced in accordance with the approved plans, any further application for residential or other non – visitor accommodation use, including seeking to extend the life of the existing permission, would need to demonstrate compliance with the IDP Policy on loss of Visitor Accommodation. This would include demonstrating that it is not financially viable to undertake works required to meet the standard for the type of accommodation and return a reasonable operational profit and that the establishment has been actively and appropriately marketed for sale and for lease for a

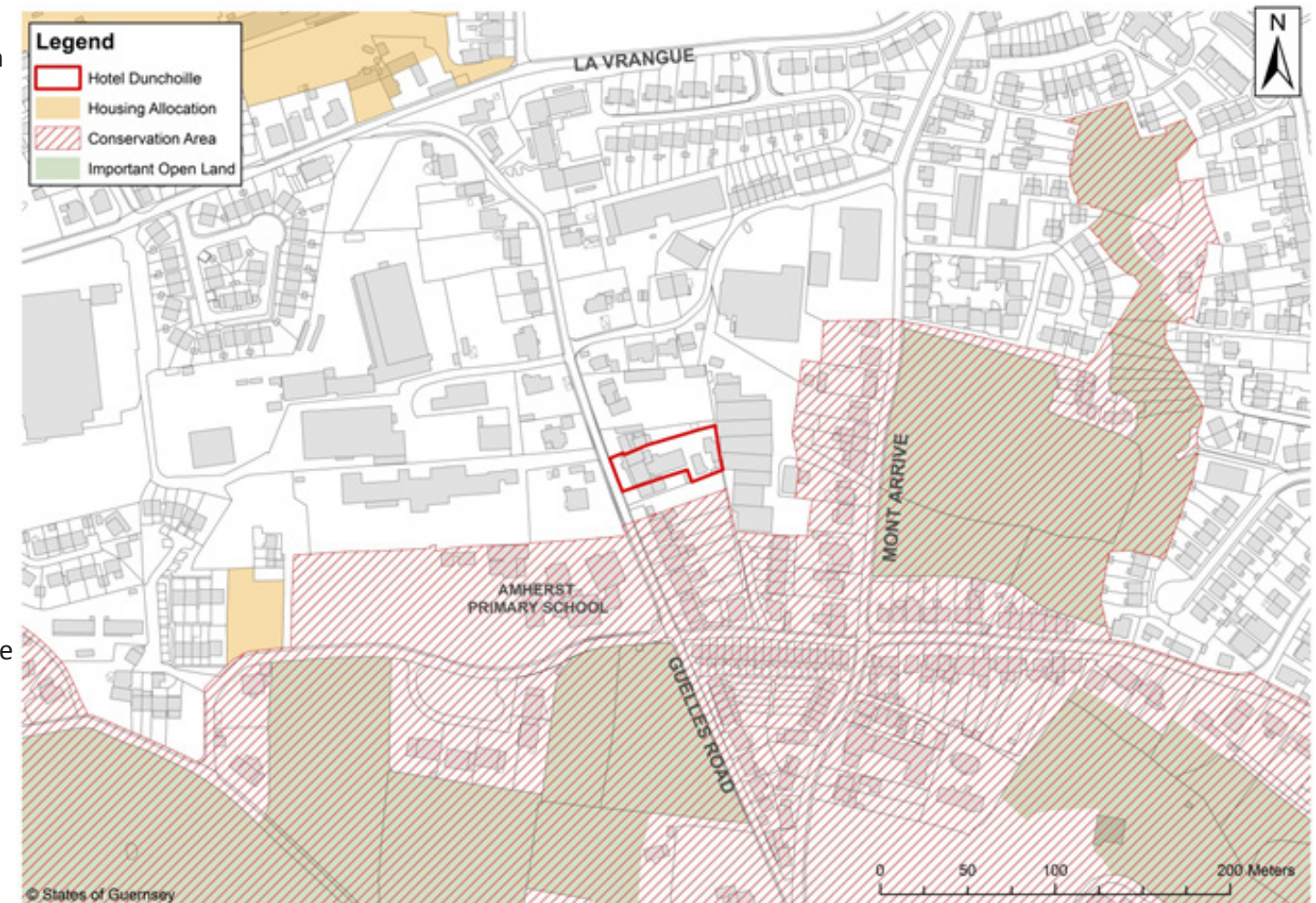


Image 1: Extract from IDP Proposals Map showing Hotel Dunchoille outlined in red

period of 24 consecutive months. It would also need to be demonstrated that it is not technically feasible to refurbish, extend, alter, redevelop or otherwise adapt the establishment to meet the standard for the type of visitor accommodation, before the principle of residential or other non-visitor accommodation use can be accepted. This would be in accordance with Policy MC8: Visitor Accommodation in Main Centres and Main Centre Outer Areas.

3.4. This Development Framework therefore provides planning guidance for residential use of the site, and, in accordance with policy MC2: Housing in Main Centres and Main Centre Outer Areas, and is required to guide proposals of 10 or more dwellings. In itself this Development Framework does not require that planning applications for residential development on this site are for 10 units or more, but given the site history, a framework is required.

3.5. Whilst individual policies of the Island Development Plan (IDP) should not normally be read in isolation or taken out of context, the policies that are considered to be particularly relevant to the site are listed and summarised in Appendix 1.

3.6. There is also relevant Supplementary Planning Guidance (SPG) for Parking Standards and Traffic Impact Assessment, as well as for Change of Use of Visitor Accommodation and for Affordable Housing.

4. Surrounding Area

4.1. The site is located in a built-up area on the outskirts of St Peter Port. There are no protected buildings, protected monuments or protected trees on site or in the surrounding area and the site lies outside the St. Peter Port Conservation Area. No sites of archaeological importance have been identified in the local area, and there are no Sites of Special Significance or Areas of Biodiversity Importance, though this does not mean that there is no potential for archaeological finds or biodiversity on site. There is no identified risk of flooding to the site.

4.2. The site is well connected to the wider road network, with access to Admiral Park and the Inter Harbour Route, providing access to The Bridge and Town. Guelles Road itself is one-way from Guelles Lane northwards, and two-way from Guelles Lane southwards.

4.3. There are several bus stops within a few minutes' walk of the site, with services to Town, L'Islet and Le Friquet and there are footpaths on both sides of Guelles Road. Beau Sejour, Cambridge Park and Candie Gardens are within walking distance of the site, and Amherst Primary School is within a 2 minute walk and St Sampsons High School a 15 minute cycle ride (a school bus picks up locally). The main town centre is approximately 15-20 minutes' walk, and Admiral Park 15 minutes away on foot.

4.4. As shown in the Nolli diagram (image 2), there is a strong pattern of development to the east of

Guelles Road, and more fragmented development to the west. Development also changes travelling north along Guelles Road, with the uniform rhythm set by the semi-detached properties becoming more broken, with a less dense pattern of development occurring. There are also areas of development with large floorplates, noticeably the industrial area to the east of the site, and the educational and commercial units to the west of the site. There is a general absence of backland development



Image 2: Nolli diagram showing pattern of development in the area



Amherst School, Guelles Road



Hotel Dunchoille in context



Image 3: Location of photographs

5. Immediate Area

Neighbouring Land Uses

5.1. Immediately to the east of the site (beyond the rear boundary) are densely developed industrial warehouse buildings with rendered blockwork walls and corrugated asbestos roofing. These buildings occupy the full extent of the eastern boundary and are built almost adjoining, forming a physical enclosure to this end of the site.

5.2. Adjoining the site to the north and south are Victorian 2 storey residential dwellings. Opposite, on the west side of Guelles Road are lower density residential dwellings. The streetscape of Guelles Road is largely made up of 2½ storey detached or semi-detached Victorian houses with front gardens facing the road.

Local Characteristics

5.3. The surrounding development has the following characteristics:

- Development on the western side of the road is generally enclosed by low granite walls with hedges and mature trees providing screening;



1. Looking south along Guelles Road from Hotel Dunchoille



2. Looking north along Guelles Road from Hotel Dunchoille

- The eastern side of the road is a combination of low walled gardens and hardstanding parking areas in front of the houses to the south of the site, with walled gardens and soft landscaping in front of the houses to the north;
- Amherst Primary school is a local landmark, one of a number of early 20th Century school buildings constructed across Guernsey, with a similar distinctive style;
- The properties along the eastern side of the road are generally semi-detached, with larger properties on larger plots just beyond the property immediately to the north of the site;
- The character of buildings to the west is very different, with the buildings set further back from the road in much larger sites and covering a larger floor area partly due to the different use of these buildings;
- The character of the area is classed as built-up, with spaces enclosed by a mixture of buildings and boundary walls, short range views, and a mixture of medium to dense development.



3. Larger properties on east side of Guelles Road beyond Hotel Dunchoille travelling north



4. Industrial units (looking north in industrial estate)



5. Neighbouring property to the north (left) of the hotel

Materials

5.4. Generally the houses in the area are a painted render finish, with slate roofs. Timber sliding sash windows have typically been replaced with UPVC. Expressed quoins are commonly used only on the street frontage of the properties. There are a number of properties with projecting bay windows on one or two storeys, some with corresponding gable roof forms presented to the street frontage. Some typical detailing is shown in the photos from the area.

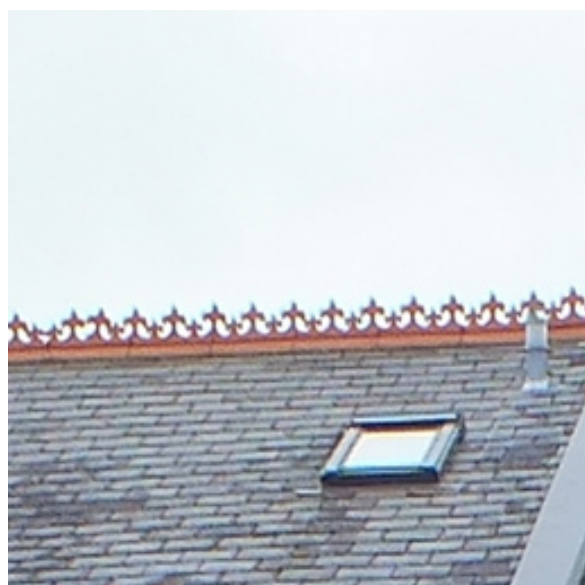
5.5. Painted render is the predominant finish to walls in the immediate area, with some ornate detailing and moulding to window cills and surrounds. Slate pitched roofs are the norm to the residential properties along the road, with the use of gables and bay windows to articulate the street frontage. Consideration of quoins to the street elevation should be given as a method of further articulating these elevations. Sliding sash windows are predominantly used along the street, many are now UPVC replacements, however the tall vertical proportions of these should be considered in adding to the harmony of the streetscape.



1. & 2. Quoins, typical in the area



5. & 6. Decorative entrance canopies



6 3. Roof detail



4. Gable roof form above two storey bay window



7. Single storey bay window



8. Two storey bay window

6. Site Description

Current Use

6.1. The site is currently occupied by a hotel and associated development. The main building was originally a house which has been extensively extended over time, with large dormers to the roof, a two storey flat roof extension to the north, and a single storey flat roof extension across the full length of the building frontage.

6.2. To the rear, the building has been extended with a two storey low pitched roof extension and a series of flat roof single and two storey extensions. The site also contains extensive hardstanding areas and a swimming pool. The aerial photo (Image 4) shows the site.

6.3. The current building on the site is forward of the building line along the street and the density is greater than the surrounding residential development.

Existing landscaping

6.4. There is some limited soft landscaping on site which comprises a small lawn area and ornamental plant varieties.

Boundaries

6.5. The north boundary of the site is formed by the neighbouring property (a two/2.5 storey dwelling) and a fence approximately 1.8 metres high. The boundary to the west (that is, the frontage of the site on Guelles Road) is the current parking area. The southern boundary consists of walls, hedging and fencing between 1.8 metres and 2 metres high, with a portion of hedging approximately 3 metres high. The eastern boundary is formed by a wall approximately 2 metres high with the corrugated asbestos clad industrial units immediately beyond with a ridge height of approximately 6.7 metres.

Site Access

6.6. A pedestrian access gate to the south west provides access into the site. Vehicle access is from Guelles Road onto the parking area in front of the existing building.

Land Levels

6.7. The site generally falls from south to north by approximately 600mm, and from east to west by approximately 700mm. The site is generally the same level as the adjoining land on all sides, with no significant changes in level between the neighbouring sites

Public Utilities

6.8. All main utility services are located in Guelles Road, including gas, water, electricity, telecoms and mains drains, with sufficient capacity to support development on the site.



Image 4: Aerial photograph of Hotel Dunchoille site, showing the buildings and swimming pool with industrial area to the east.



1. Frontage of Hotel Dunchoille, facing Guelles Road



2. Rear of Hotel Dunchoille, looking west



3. East boundary looking south, with industrial unit visible beyond



4. Southern boundary, eastern end



5. Looking east to rear boundary of the site



6. Looking west along southern boundary

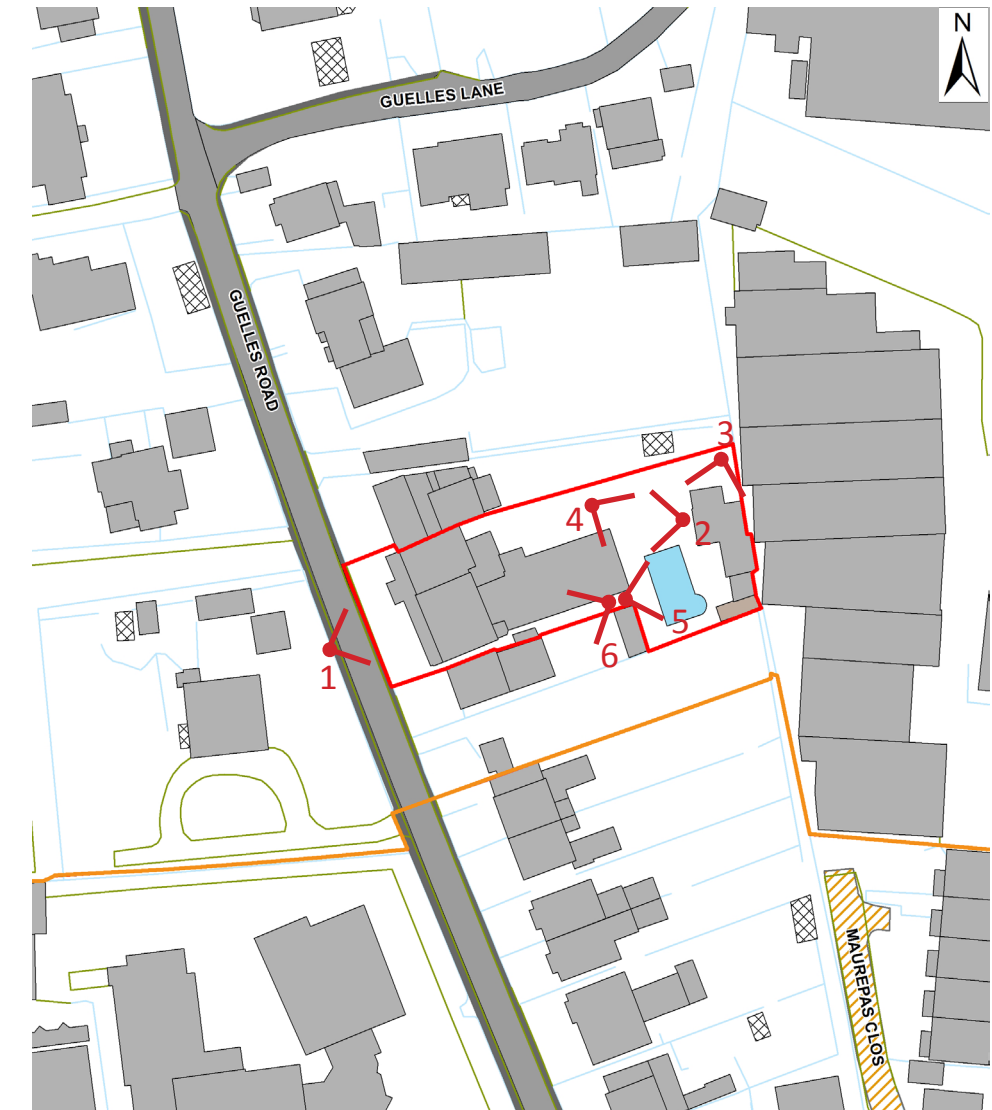


Image 5: Location of photographs

- Site Boundary
- Existing access
- Parking
- Single storey
- Two/ two and a half storey
- Conservation Area
- Trees
- Neighbouring Gardens
- Lawn within site
- Hardstanding
- Pool

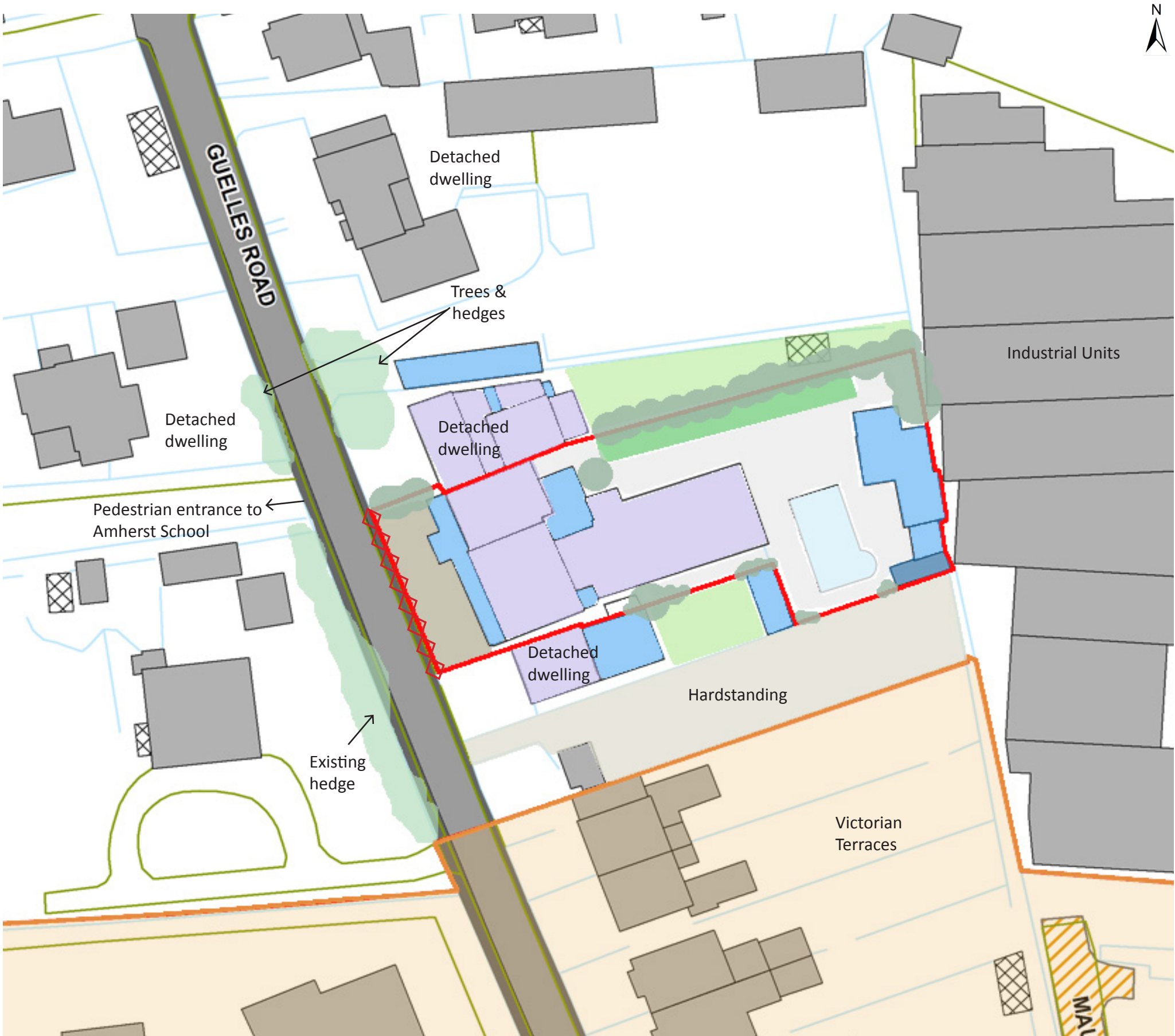


Image 6: Site description

7. Site Analysis

7.1. Proposals for development need to take into account the constraints and opportunities that the site presents. For this site, there are significant constraints as well as opportunities to improve the existing site:

- The relationship of built development on the site with the buildings on its three developed sides will need careful consideration;
- There may be opportunities to improve the current layout where there is significant overlooking of neighbouring properties to the north and south of the site;
- New development must consider potential overshadowing and overlooking to the neighbouring dwellings;
- There is limited outlook to the east of the site where industrial units are close to the boundary;
- The industrial buildings to the east may constrain the design of development as a result of their form;
- An opportunity exists to create a street frontage more in keeping with the neighbouring properties on Guelles Road, respecting the scale, mass and nature of the streetscape;
- Biodiversity of the site is currently low, and a comprehensive landscaping scheme as part of new development could improve biodiversity of the site;
- Taking into account the constraints identified there may be options to adjust the existing site levels to address some of the overshadowing issues;
- There are a variety of distinctive characteristics that could influence design on the site, such as decorative quoins, entrance canopies, bay windows and tall vertical window proportions;
- Historically there were a number of brickworks located in this area and there may be some potential to use some brickwork in the design;
- There is potential to reduce the plot ratio, massing and volume from what currently exists on the site.

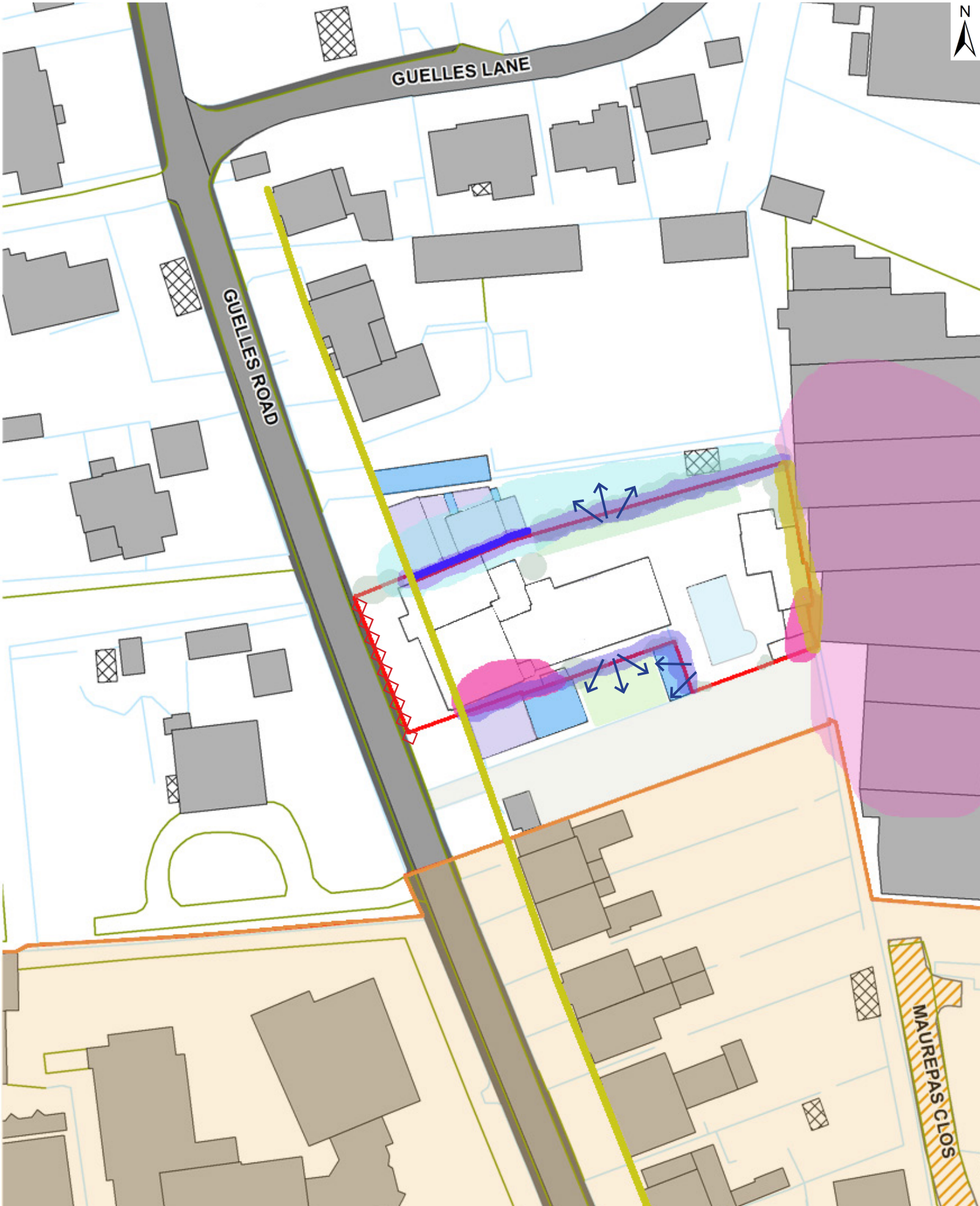


Image 7: Site analysis

8. Development Guidelines

Type of development

8.1. The site is located within the St Peter Port Main Centre Outer Area, where IDP policies allow for a range of uses. However this Development Framework only considers housing, and proposals for other development types would be considered against the relevant policies in the IDP.

8.2. Policy MC2: Housing in Main Centres and Main Centre Outer Areas requires an appropriate mix and type of dwellings to be provided. The mix and type of dwellings should be reflective of the demographic profile of households requiring housing based on the most up to date evidence available. The exact mix will be determined at the point of any planning application on the site, informed by the most up to date information such as Housing Needs Studies and other relevant information held by the States of Guernsey.

8.3. Developments which result in a net increase of 20 or more dwellings are required to provide a proportion of the developable land for affordable housing in accordance with Policy GP11.

Density and Housing Numbers

8.4. The site is approximately 0.13 hectares (1 vergée) and the current buildings on the site cover approximately 980m² (0.09 hectares, equivalent to approximately 72% of the site). The site currently has Planning Permission to convert the Hotel into 9 flats with a separate staff accommodation block which is valid until 16 June 2018 and which retains the existing built form.

8.5. To achieve 10 or more dwellings on this site would require a density range equivalent to around 80 dwellings per hectare. As noted before, this development framework in itself does not require that proposals meet this density, but nor does it exclude that possibility. All planning applications for residential development on this site are expected to accord with this Development Framework. The exact number of units on site will depend on the detailed design response to the specifics of this site, including provision of appropriate amenity space proportionate to the type of housing proposed, the provision of parking, and the consideration of residential amenity. This may result in an acceptable development of more, or less, than 10 units on the site.

Design

8.6. To achieve development on this site, proposals will need to be of a particularly high standard of design to overcome the constraints of the site and to take advantage of the opportunities to improve on the existing. Any planning application will need to demonstrate a detailed understanding of the site and the effects of development on properties to the north and south and on the streetscene. The proposed design should respect and enhance the form, scale and mass of the existing streetscape. Sustainable Urban Drainage (SUDs) interventions are required to ensure that surface water run-off from the site is properly controlled. Policy GP18: Public Realm and Public Art should also be reflected in the design of any scheme, where the inclusion of appropriate public art as an integral part of the scheme is encouraged.

8.7. Development should result in an enhanced street frontage following the building line along Guelles Road, with building form and scale in keeping with the surroundings. Options may exist which take into account constraints to carefully develop the rest of the site behind the front block, and these options must include a sensitive response to the identified issues of overlooking, overshadowing and outlook, both in terms of existing residents of neighbouring properties and also residents of any new development on site.

Materials

8.8. The architectural appearance of the development should draw from and respond to the character of the surrounding buildings, though design need not replicate the surroundings. Consideration should be given to the finishes and materials as well as typical forms of detailing used in the area, whilst not precluding the use of contemporary detailing and use of familiar materials to the streetscape. There may be opportunities to consider alternative materials inside the site to create a harmonious contrast to the streetscape.

Landscaping

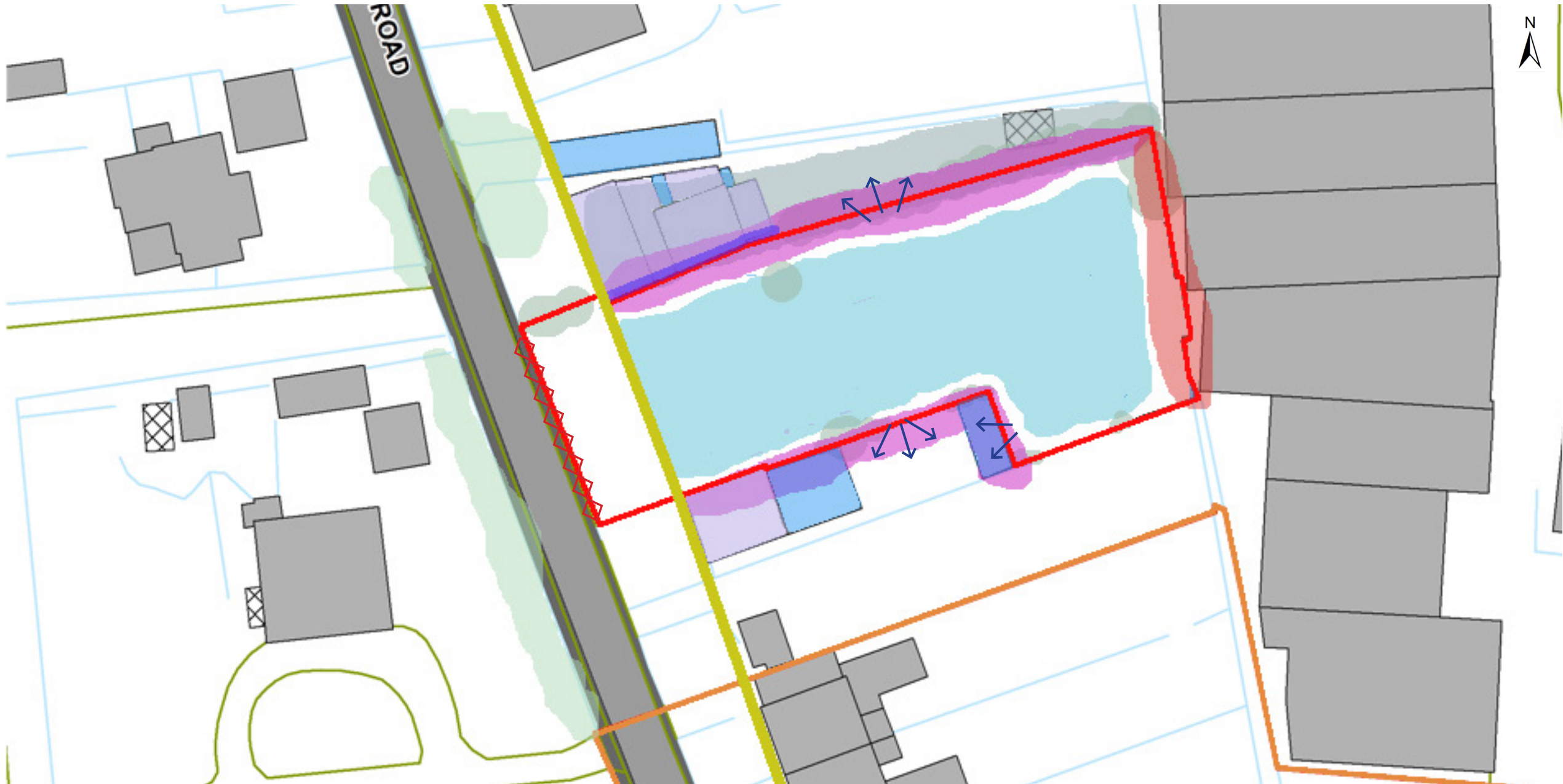
8.9. In order to enhance the amenity of the residents on the site and to enhance the potential biodiversity of the area planning applications relating to the site should incorporate a carefully considered landscaping scheme of both soft and hard landscaping. The landscaping needs to be fully integrated into the overall scheme design to ensure a suitable external environment is created that will improve the natural habitat of the area, whilst providing for the needs of the residents in the form of a practical design that will develop comfortably over time. Appropriate species for tree planting may include Ash, Silver Birch, Sweet Chestnut, Hornbeam, English Oak, Evergreen Oak, Scot's Pine. Appropriate hedging species may include Blackthorn, Elder, Gorse, Hawthorn, Holly.

Access Arrangements

8.10. Guelles Road is the only suitable location for an access point to the site. The proposed design must provide for vehicular access from Guelles Road with suitable visibility splays and carriageway width to satisfy the requirements of Traffic and Highway Services. Safe access must also be provided for pedestrians and cyclists to the site, this could take the form of a shared surface so long as it is suitably designed.

Parking Requirements

8.11. In accordance with policy IP7: Private and Communal Car Parking, and Parking Standards Supplementary Planning Guidance, appropriate levels of parking must be provided on site for cars, motorcycles and bicycles. However the strict application of standards can sometimes have an undesirable impact upon the appearance or function of a particular development and although the provision of parking should be expected to comply with Supplementary Planning Guidance: Parking Standards and Traffic Impact Assessment, it will be interpreted flexibly where it is considered that a better overall development can be achieved. The parking standards are set out on pages 6 and 7 of the Supplementary Planning Guidance. Cycle parking should be considered at the outset of the design process and be covered, secure and easily accessible in order to comply with policy IP6: Transport Infrastructure and Support Facilities. Proposals must also demonstrate compatibility with policy IP9: Highway Safety, Accessibility and Capacity where the road network must be able to cope with the increased demand resulting from the new development.



	Site boundary		Existing access		Conservation area		Amenity constraint due to proximity of neighbouring development
	Building line		Trees		Indicative developable area		Windows on elevation overlooking site
	Single storey		Consideration of overshadowing		Consideration of overlooking		
	Two/ two and a half storey						

Image 8: Development guidelines

9. Waste Management Plan and Construction Environmental Management Plan

9.1. Development Frameworks are required to include an outline Waste Management Plan specific to the site in question, a detailed Waste Management Plan is required at planning application stage.

9.2. Waste Management Plans apply to all aspects of a project, with the majority of opportunities for waste minimisation existing at the design phase. Information should be provided with a planning application on the amount and type of waste that will be produced during the course of a project and how waste will be reduced, reused, recycled, recovered or disposed of. This should be by means of a living document, drafted up from the conception of a project and being added to and evaluated until the completion of the development and submitted again to the Authority prior to occupation or use of any dwelling on the site.

9.3. All materials from the demolition of the existing buildings will be carefully sorted, separated, and distributed accordingly through the appropriate routes for recycling, recovery or disposal, in order to minimise the waste produced.

9.4. The final design will incorporate dedicated waste and recycling storage provision on the site suitable to the density of the site.

9.5. A Construction Environmental Management Plan (CEMP) is also required to be submitted with a planning application. It should consider environmental issues such as contamination, ground water and ecology. It should also consider and minimise the effects on neighbouring properties resulting from the demolition and construction phase, and include issues such as noise, vibration, dust and construction traffic.

IDP Policy	Policy Relevance
S1: Spatial Policy	The Island Development Plan's spatial policy is for the Main Centres and Main Centre Outer Areas to have the role as the focal point for development to maintain the vitality of these areas.
S2: Main Centres and Main Centre Outer Areas	Proposals for development in the Main Centre Outer Areas will generally be supported where this would not detract from the Main Centres as the core focus for economic and social growth.
MC2: Housing in Main Centres and Main Centre Outer Areas	This policy requires the Development Framework for the site which, once approved, will be taken into account when considering proposals for the site. Where a variety of dwelling types can be accommodated, there should be an appropriate mix and type which will be reflective of the demographic profile of households needing housing.
MC8: Visitor Accommodation in Main Centres and Main Centre Outer Areas	This policy requires applicants to demonstrate that it is not financially viable to undertake works required to meet the standard for the type of accommodation and return a reasonable operational profit and that the establishment has been actively and appropriately marketed for sale and for lease for a period of 24 consecutive months, or that it is not technically feasible to refurbish, extend, alter, redevelop or otherwise adapt the establishment to meet the standard for the type of visitor accommodation before the principle of residential use can be accepted.
GP1: Landscape Character and Open Land	This policy sets out that development will be supported where it respects the relevant landscape character type within which it is set, where development does not result in the unacceptable loss of any specific distinctive features that contribute to the wider landscape character and local distinctiveness of the area and takes advantage of opportunities to improve visual and physical access to open and undeveloped land. A landscaping scheme is expected to be submitted as part of any proposal on this site.

GP8: Design	<p>Development, including the design of necessary infrastructure and facilities, is expected to achieve a high standard of design which respects, and where appropriate, enhances the character of the environment.</p> <p>Two or more storey buildings constitute a more efficient use of land than single storey buildings and therefore development proposals should consider a multi-storey design from the outset, unless there are overriding reasons why this design approach would be unacceptable. Proportionate residential amenity space must be provided appropriate to the housing type and location. Development must respect the character of the local built environment and provide soft and hard landscaping to reinforce local character and/or mitigate the impacts of development including contributing to more sustainable construction. The amenity of occupiers and neighbours is also important – see IDP Annex I for further information. Residential accommodation is required to be accessible for all and to be flexible and adaptable. Proposals will need to demonstrate that they have been designed in such a way that design features that support people being able to live in their own homes as long as possible can be easily added in the future when required.</p> <p>Development should also provide adequate individual or communal areas for storage of refuse and recyclable materials</p>
GP9: Sustainable Development	<p>The Policy is wide-ranging and includes requirements for sustainable design and construction with reference to the design, layout and orientation of buildings, flood risk and surface water run-off, renewable energy and the use of materials.</p> <p>Development of 5 or more dwellings will require a Waste Management Plan. It should consider the re-use/disposal from demolition of any existing buildings on site.</p>
GP10: comprehensive Development	Any proposal will be required to be a comprehensive development and make the most effective and efficient use of the land.

GP11: Affordable Housing	This policy requires proposals for development resulting in a net increase of 20 or more dwellings to provide a proportion of the developable area of the site for affordable housing.
GP18: Public Realm and Public Art	Any proposal should consider the relationship between the development and the public realm and should enhance where possible.
IP1: Renewable Energy Production	Proposals for renewable energy installations (and ancillary and associated development) will be supported where they can be satisfactorily incorporated into the built form of the proposed development.
IP6: Transport Infrastructure and Support Facilities	Development should encourage a range of transport options and should be well integrated with the transport network. Development is required to provide appropriate levels of bicycle and motorcycle parking in accordance with the Supplementary Planning Guidance: Parking Standards and Traffic Impact Assessment.
IP7: Private and Communal Car Parking	The parking standards for the IDP are set out in the Supplementary Planning Guidance: Parking Standards and Traffic Impact Assessment. For residential development in the Main Centre outer Areas the car parking standards are in section 6. The standards are maximums and the development would be expected to be within these standards. Provision will need to be made for secure covered bicycle parking for residents and visitors – refer to SPG paragraph 5.3. Provision for motorbikes is needed if there are at least five car parking spaces – refer to SPG paragraph 4.1.
IP9: Highway Safety, accessibility and Capacity	The DPA will consider the impact of development on the public road network and the access requirements of people of all levels of mobility.

Contact Us for further information and advice at: Planning Service, Sir Charles Frossard House, St Peter Port. GY1 1FH Telephone 01481 717200 Email planning@gov.gg

Have you visited our website? Go to www.gov.gg/planning_transport_waste_services for additional guidance material and other planning information, including how to book a pre-application discussion.

This SPG is issued by the Development & Planning Authority to assist understanding of the provisions of the Island Development Plan (2016) and, where applicable, relevant legislation, by offering detailed guidance but is not intended to be exhaustive or a substitute for the full text of legislation or the policies within the Island Development Plan (2016). This SPG does not form part of the Island Development Plan (2016). It represents the Development & Planning Authority's interpretation of certain provisions of States of Guernsey policy or legislation. The guidance is indicative of the Development & Planning Authority's likely approach to development proposals in relation to the site in question and is not binding on the Development and Planning Authority. This SPG does not prejudice the Development & Planning Authority's discretion to decide any particular case differently according to its merits and it does not relieve the Development & Planning Authority of any obligation, restriction or liability imposed by or under the Land Planning and Development (Guernsey) Law, 2005. Copies of the text of the Island Development Plan (2016) are available from Sir Charles Frossard House and also available electronically online at www.gov.gg/planningpolicy. Copies of legislation are available from the Greffe. Electronic copies are also available at www.guernseylegalresources.gg. Substantive queries concerning the guidance or a specific site should be addressed to the Planning Service by email at planningservice@gov.gg. The Development & Planning Authority does not accept any liability for loss or expense arising out of the provision of, or reliance on, any advice given. You are recommended to seek advice from an independent professional advisor where appropriate.